



# Staff Report

PLANNING DIVISION  
COMMUNITY & ECONOMIC DEVELOPMENT

To: Salt Lake City Planning Commission  
From: Casey Stewart; 801-535-6260  
Date: April 17, 2014  
Re: PLNPCM2014-00045 Conditional Building and Site design for 151 Tower Office Building

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## CONDITIONAL BUILDING AND SITE DESIGN REVIEW

**PROPERTY ADDRESS:** 145 South State Street (to be re-addressed as 151 South State St)

**PARCEL ID:** 16-06-107-035

**MASTER PLAN:** Central Community, Downtown

**ZONING DISTRICT:** D-1 (Downtown)

**REQUEST:** Approval of the proposed building and site design for a new commercial office building; specifically, the building height (18-stories, 295 feet) and front setback require additional consideration. The Planning Commission has final decision making authority for Conditional Building and Site Design Review.

**RECOMMENDATION:** Based on the information in this staff report, planning staff recommends that the Planning Commission deny the requested amendment.

### ATTACHMENTS:

- A. Vicinity Map
- B. Site Plan
- C. Building Elevations
- D. Additional applicant Information
- E. Existing Conditions
- F. Analysis of Standards
- G. Public Process and Comments
- H. Dept. Comments
- I. Motions

### PROJECT DESCRIPTION:

#### 1. Proposal Details

The proposal seeks to increase the height allowance for a proposed mid-block office tower on State Street. The building would have 18 floors above ground level and a total height of approximately 295 feet (273 to roof level with 22 feet of mechanical penthouse above the roof level). Buildings constructed in the mid-block area of downtown (D-1 zoning districts) are typically limited to 100 feet, but are allowed to go higher if they comply with the standards for Conditional Building and Site Design Review.

The building would take up virtually all of the land area, less a sidewalk on the south side and a courtyard area on the east side, between the proposed office tower and the under-construction parking structure to the east. As proposed, the building faces State Street, is predominantly glass, and will accommodate vehicle parking by sharing the adjacent (east) parking structure and some stalls in the existing federal government parking area immediately north of the site.

**KEY ISSUES:**

The key issues listed below have been identified through the analysis of the project, neighbor input and department review comments.

**Issue 1: Roofline architecture - unresolved**

The original roofline architecture was very similar to other recent office tower projects at 222 South Main Street (completed) and 111 South Main Street (under construction). These buildings have flat rooflines consisting of glass parapets. A district roofline is a criterion when seeking extra building height. Based on initial planning staff concerns of an indistinct roofline (when compared to those recent projects), the applicant modified the roofline in an attempt to meet the criterion; however this latest roofline version does not sufficiently distinguish itself from the other projects, or the original roofline design – especially for the amount of extra height requested.

The building design overall uses varying angles and cuts to create subtle visual illusions of building form; and the proposed roofline continues this theme, but its similarities with other recent buildings remains readily apparent. Both designs proposed by the applicant are included with this report for the planning commission's consideration. Lacking more distinct roofline architecture, planning staff cannot fully recommend in favor of the extra building height requested, as discussed in the analysis that follows.

**Issue 2: Mid-block walkway - resolved**

The city is prioritizing mid-block walkways more throughout downtown blocks to provide access choices to pedestrians and cyclists, to improve pedestrian connectivity. The location of this project in the mid-block area offers an opportunity to develop a mid-block walkway; however the proposed building would occupy almost the entire site, potentially reducing the options for a viable mid-block connection. Additionally, the parking structure that is being constructed on the property to the east is a Boyer Company project, the same company developing this office tower. The parking structure was approved last year as a permitted use and did not require any additional planning division review and no attention was given to a mid-block walkway.

The vehicle access to the parking structure can serve as a pedestrian connection but there is great potential for pedestrian/vehicle conflict. The applicants are working on an agreement with the federal government, owners of the parcel north of the subject site, to maintain existing vehicle access from State Street to the parking garage drive, thus allowing for a complete vehicle thru-way between State and 200 East streets. The applicant worked with city staff to create a pedestrian sidewalk along the north edge of the parking structure and designated a pedestrian route between State and 200 East, achieving the city's goal for prioritized pedestrian access through this block (mid-block walkway). A portion of the vehicle access on the north side of the parking structure would be signed as "one-way" to reduce pedestrian conflict, particularly with larger service vehicles.

**DISCUSSION:**

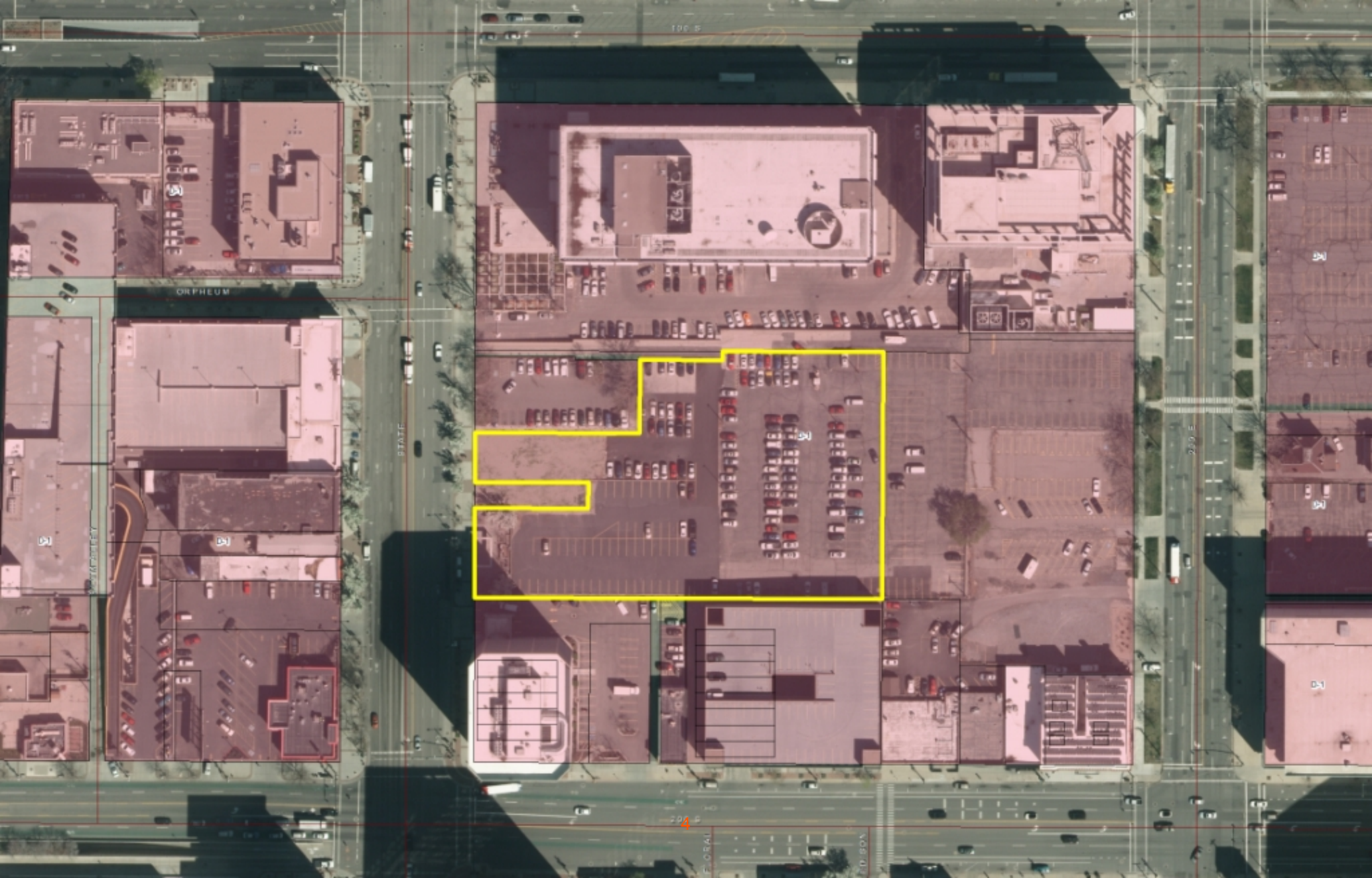
In general the proposal is well thought out and satisfies most of the design standards for approval. The building, despite being tall, is designed with a ground level that is pedestrian focused and visually interesting with columns, expansive glass looking into an active lobby area, and various angular cuts vertically along the corners. Proposed vehicle access is subdued by proposed landscaping, while the existing parking lot area to the north would be converted to a loading/drop-off area for the new building, thereby reducing the number of vehicles in public view. The proposal satisfies all of the general design standards, but does not conclusively satisfy the specific design standards for extra height – in particular the roofline architecture. That one aspect results in staff's decision to not recommend approval of the extra building height; otherwise staff supports the rest of the project.

**NEXT STEPS:**

If approved, the applicant may proceed with the project and will be required to obtain all necessary permits. If denied the applicant would still be able to construct a building but it would be limited to 100 feet tall, per the basic mid-block development standards.

## **ATTACHMENT A: Vicinity Map**

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ORPHEUM

STATE

239 E

FLORAL

2050 S



## **ATTACHMENT B: Site Plan**

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## **ATTACHMENT C: Building Elevations**

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**ORIGINAL DESIGN**



The building's main entrance on State Street exhibits a monumental scale highlighting the building's grand lobby space. While situated mid-block on the street, the building has a generous separation from adjoining structures which preserves views on all sides.



**ORIGINAL DESIGN**



Viewed from the intersection north of the site, the building has full exposure of both the North and West facades. The slices carved off the corners of the building yield an elegant tapered appearance.



**ORIGINAL DESIGN**



From the south, the building appears to widen at the top due to the sculptural clipping of the opposing corners. As an artistic object the building has a distinct appearance when viewed from different angles.



ORIGINAL DESIGN



151

RENDERING

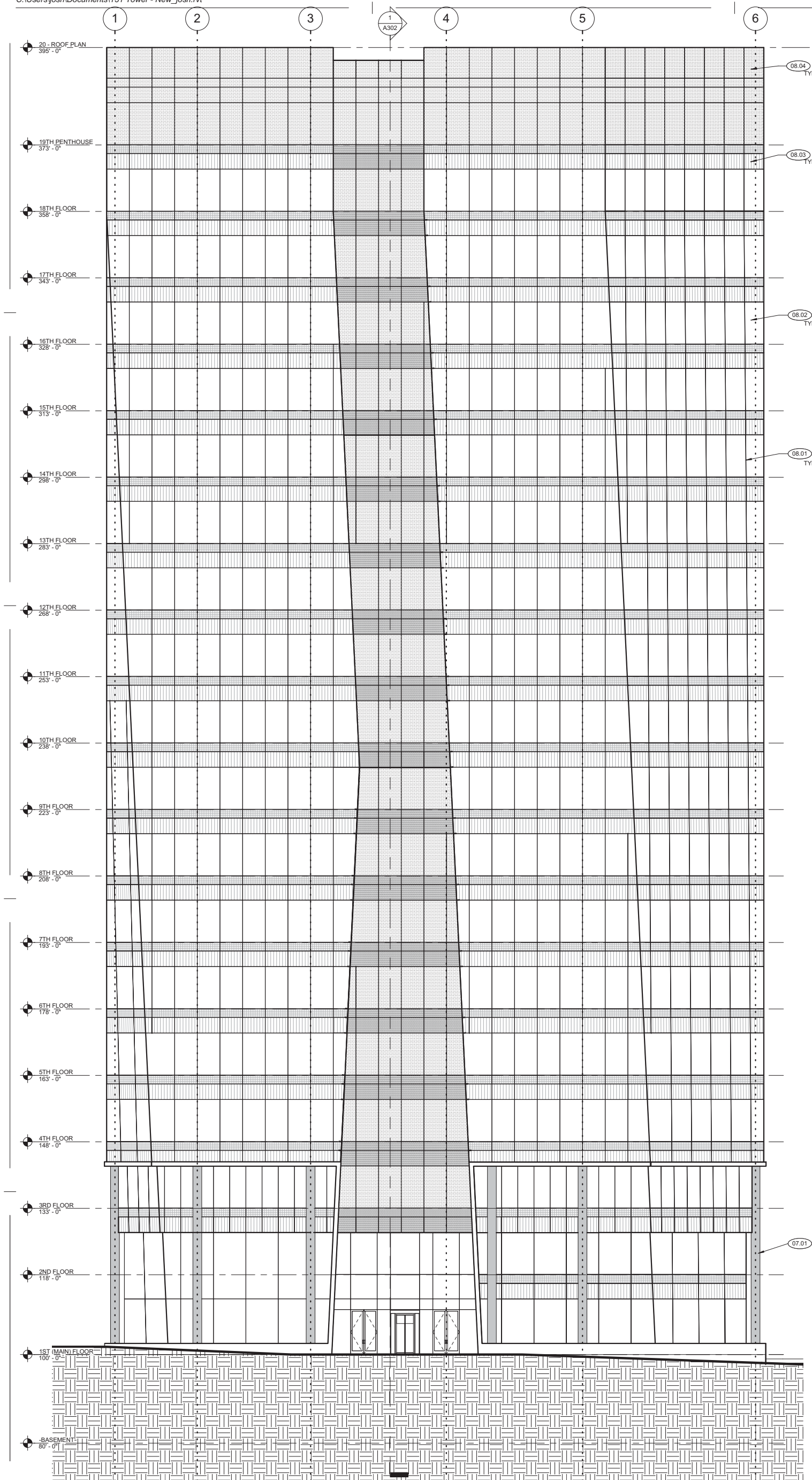
Viewed from the state street entrance of the City Creek mall just North of the site, the building stands majestically above the neighboring buildings.



**REVISED PARAPET DESIGN**







**REFERENCE NOTES**

- 07.01 COLUMN ENCLOSURE, TBD.
- 08.01 CURTAINWALL SYSTEM, SEE SPECS.
- 08.02 VISION GLAZING, SEE SPECS.
- 08.03 SPANDREL GLAZING, SEE SPECS.
- 08.04 FRITTED GLAZING, SEE SPECS.

**WINDOW GLAZING TYPES**

- A 1/2" CLEAR GLAZING
- B 1/2" TEMPERED GLAZING
- C 1/4" TEMPERED GLAZING

**1 WEST ELEVATION**  
 A201 scale: 3/32" = 1'-0"

**WEST ELEVATION**

**A201**

#	Date	Description

ISSUE: 12.10.2013  
 PROJECT NO: 13027  
 DRAWN BY: JH  
 CHECKED BY: DD  
 SHEET TITLE: WEST ELEVATION

PRELIMINARY

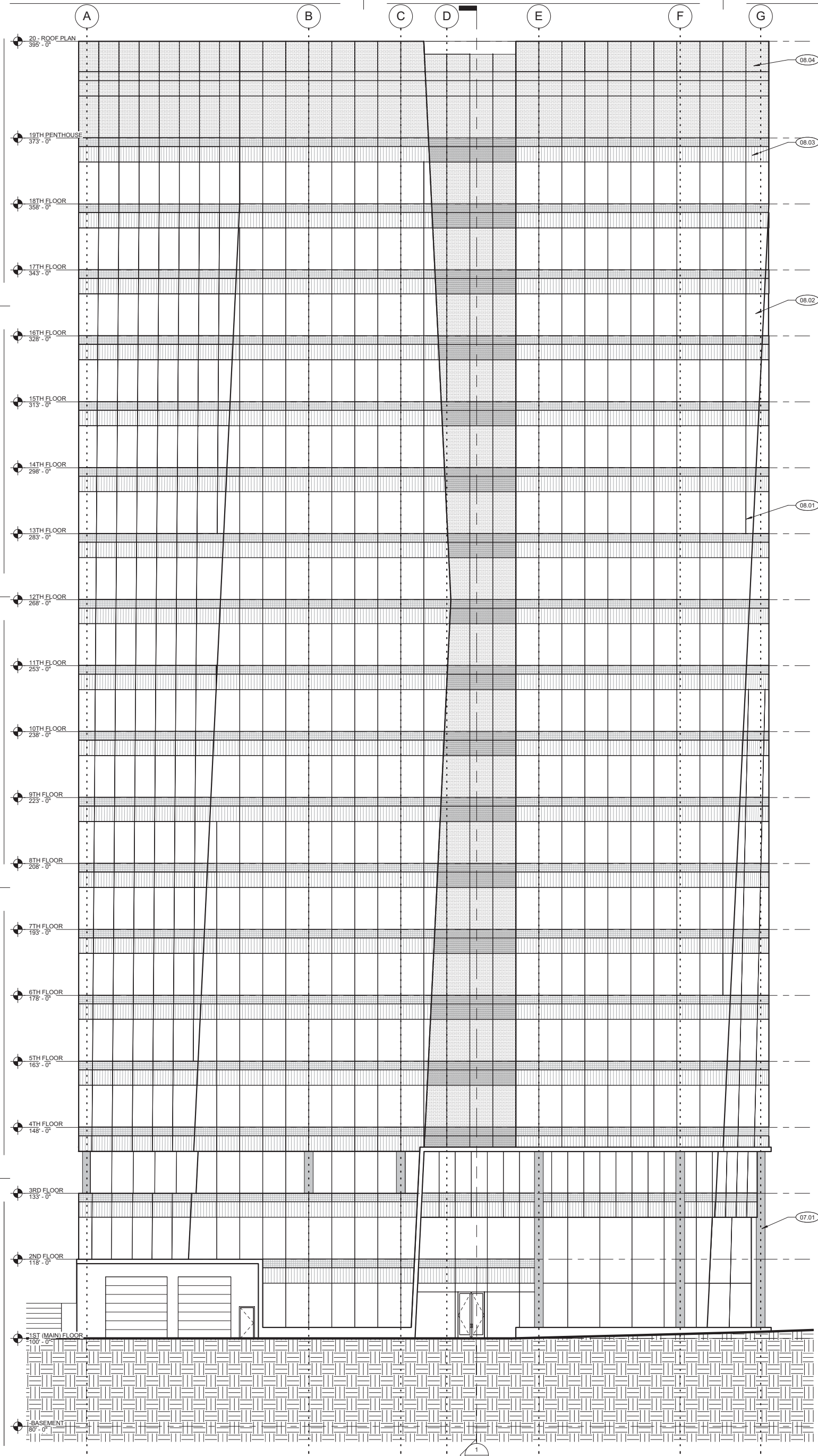
151 SOUTH STATE  
 STREET,  
 SALT LAKE CITY, UT  
 84111

**151 TOWER**  
 13

**BOYER**

**dixon**  
 833 south 200 east  
 salt lake city, ut 84111  
 phone 801.595.6400  
 www.dixoninc.com





- REFERENCE NOTES**
- 07.01 COLUMN ENCLOSURE, TBD.
  - 08.01 CURTAINWALL SYSTEM, SEE SPECS.
  - 08.02 VISION GLAZING, SEE SPECS.
  - 08.03 SPANDREL GLAZING, SEE SPECS.
  - 08.04 FRITTED GLAZING, SEE SPECS.

**1 NORTH ELEVATION**  
 A202 scale: 3/32" = 1'-0"

#	Date	Description

ISSUE: 12.10.2013  
 PROJECT NO: 13027  
 DRAWN BY: JH  
 CHECKED BY: DD  
 SHEET TITLE: NORTH ELEVATION

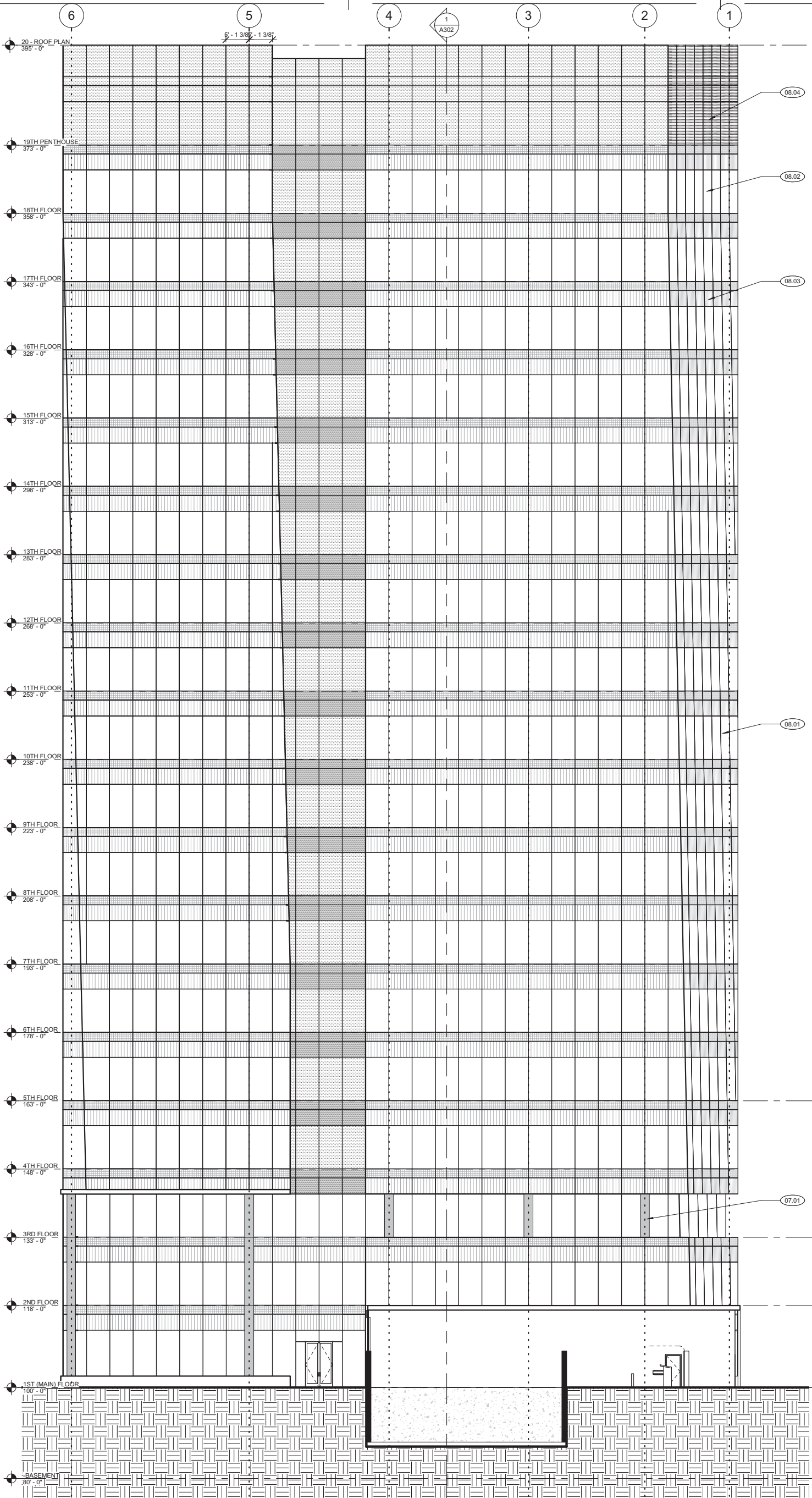
PRELIMINARY

151 SOUTH STATE STREET,  
 SALT LAKE CITY, UT  
 84111

151 TOWER  
 14







**REFERENCE NOTES**

- 07.01 COLUMN ENCLOSURE, TBD.
- 08.01 CURTAINWALL SYSTEM, SEE SPECS.
- 08.02 VISION GLAZING, SEE SPECS.
- 08.03 SPANDREL GLAZING, SEE SPECS.
- 08.04 FRITTED GLAZING, SEE SPECS.

**1 EAST ELEVATION**  
 A203 scale: 3/32" = 1'-0"

#	Date	Description

ISSUE: 12.10.2013  
 PROJECT NO: 13027  
 DRAWN BY: JH  
 CHECKED BY: DD  
 SHEET TITLE: EAST ELEVATION

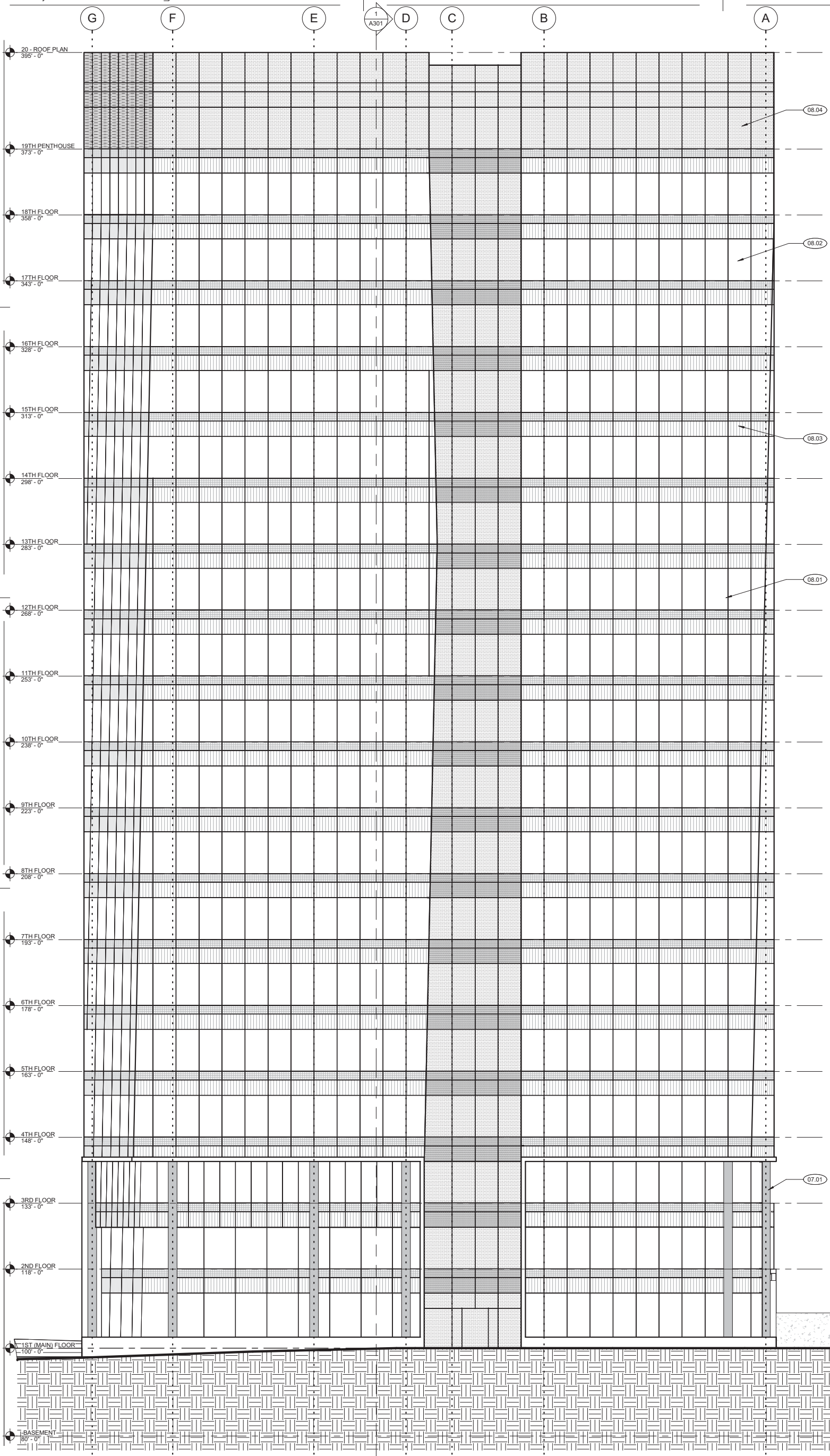
PRELIMINARY

151 SOUTH STATE STREET,  
 SALT LAKE CITY, UT  
 84111

151 TOWER 15



A203



- REFERENCE NOTES**
- 07.01 COLUMN ENCLOSURE, TBD.
  - 08.01 CURTAINWALL SYSTEM, SEE SPECS.
  - 08.02 VISION GLAZING, SEE SPECS.
  - 08.03 SPANDREL GLAZING, SEE SPECS.
  - 08.04 FRITTED GLAZING, SEE SPECS.

**1 SOUTH ELEVATION**  
 A204 scale: 3/32" = 1'-0"

#	Date	Description

ISSUE: 12.10.2013  
 PROJECT NO: 13027  
 DRAWN BY: JH  
 CHECKED BY: DD  
 SHEET TITLE: SOUTH ELEVATION

**PRELIMINARY**

**151 TOWER**  
 16

151 SOUTH STATE STREET,  
 SALT LAKE CITY, UT  
 84111



**dixon**  
 ARCHITECTURAL FIRM

833 south 200 east  
 salt lake city, ut 84111  
 phone 801.595.6400  
 www.dixon-arch.com



## **ATTACHMENT D: Additional Applicant Information**

## Boyer 151 Tower Project Description

The proposed project consists of an 18 story office building with a full basement and roof top penthouse. The building footprint is approximately 22,000 sq. ft. and the building height will be 273 feet to the floor level of the mechanical penthouse with the penthouse extending approximately 22 feet in height.

The construction will be Type I-A in accordance with IBC Chapter 6 modified to Type I-B for everything except building columns by IBC Chapter 4 (exception 403.2.1.1.) for high rise office buildings protected throughout by an automatic sprinkler system.

The building faces State Street with a covered entrance and also has covered entrances on the other three faces of the building.

Parking consists of a eleven short-term visitor stalls on the north dedicated to the Federal Building along with a multi-tiered parking garage to the east of the building separated by an open courtyard. The first phase of the parking garage is now under construction and will be shared with the Boyer 102 Tower project which is located at 102 South 200 East. Each building will have fewer than the maximum prescribed 2.5 parking stalls per 1,000 square feet of building area. Access to the parking structure from State Street will be provided across the existing Federal Building visitor parking lot on the north side of the building via a shared parking/access agreement. The existing parking lot will be demolished and replaced as part of this project. The new driveway/curb cut onto State Street will replace the existing one-way driveway that the Federal Government currently has adjacent to their employee/delivery driveway. The new separated location will eliminate the traffic confusion that currently exists for the visitor lot and the alley that extends through to 200 East St. UDOT officials have reviewed the proposed relocation of the driveway and combined use and are in agreement with the proposed design and driveway location.

Utilities for the building will be from State Street. In addition, a new water line will be extended from the new water main in 200 East to provide an additional water supply to the building. Electrical power will also be provided from two separate sources extending from two separate sub-stations. The building will have the required backup generators and fire pumps as required by code. Generators will be located below grade and in the basement to contain sound and visibility.

The 151 Tower maintains the required setbacks for the generous areas of glass on its facades (15 feet clear area required for up to 75% of façade in unprotected openings). The required setback on the north will be achieved through a dedicated 15 feet wide no-build easement that will be provided by the property owner to the north and properly recorded.

The 151 Tower is not located on a corner, but since the adjacent Federal Building on the corner of 100 South and State Street is setback far from the intersection, the 151 Tower enjoys a view of the corner across the open plaza of the Federal Building similar to a corner building. The building to the south of the 151 building (185 South State Building) also has a larger than normal setback from the 151 Tower due to the drive-through banking lanes on the north side. As such, the 151 Tower will enjoy a setting unlike other mid-block buildings in the downtown area. The 151 Tower will exceed the prescribed maximum height of 100 feet for buildings located mid-block in the D-1 Zone so the applicant is

requesting conditional approval as allowed by ordinance based on the provisions of Chapter 21A.59 of the ordinance.

All of the requirements of Chapter 21A.59 will be met as follows:

- a. The building is oriented to the street with the primary access oriented to the pedestrian and mass transit (bus stop in front of building).
- b. The building façade is detailed with glass oriented to pedestrian interest interspersed with areas of natural stone or metal. The grand lobby enjoys a large expanse of glazing oriented to the street and pedestrian movements on the northwest corner of the building.
- c. The parking is set behind the building with appropriate cut-off lighting. All lighting will be engineered to meet Salt Lake City lighting requirements.
- d. Pedestrian access to the parking lot is via sidewalks and/or covered walkways.
- e. Loading docks are contained within the building and dumpsters are screened from view behind the building.
- f. Signage will be provided to direct pedestrians using mass transit.
- g. At least six street trees will be provided to meet the requirement of one tree for every 30 feet of frontage (165' wide lot) along with additional street trees for the short-term visitor parking lot.
- h. Landscape material will be selected to assure 80% coverage within 3 years. The front public sidewalk will be replaced as part of the construction and will consist of areas of concrete separated by brick pavers to match the City's standard for this street.
- i. The building will not be required to have a dedicated public space as prescribed by the ordinance by approval of the City Planner due to its location in the D-1 Zone. Nevertheless, an outdoor courtyard is planned for the space between the building and the parking garage to the east for use by building tenants. This courtyard will include seating, areas of shade, outdoor eating space and attractive landscaping.

We believe this building will be a beautiful addition to the Salt Lake City skyline and provide premium quality office space and amenities for those that will office here.

**From:** [Dave Dixon](#)  
**To:** [Stewart, Casey](#)  
**Cc:** [Jake Boyer](#)  
**Subject:** Proposed 151 Building - Parapet Design  
**Date:** Wednesday, April 09, 2014 12:10:39 PM  
**Attachments:** [18story-SitePlan-04-07-2014-FrontView-11.png](#)

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Casey,

We played around with a dozen options this week for the parapet for 151 which included cutting off the glass parapet around the building at a slight angle, extending the penthouse taller than the glass parapet to create a step in the façade similar to the step in the façade from the third floor to the fourth floor, different shapes for the penthouse itself, sloping just the top of the penthouse and exposing it above the glass line, etc., etc. We only came up with one option that we feel can meet the intent of the ordinance and strengthen rather than compromise the design. (See attachment)

The modified design involves three revisions from the original design submitted to the City:

1. Modulate the top of the parapet more by greatly increasing the vertical depression at the niches. We originally had two feet drops in the top of the glass parapet at the niches and now we are proposing six feet. With three times more variation in the top of the building cornice line than we had in original design, it strongly accentuates the four quadrants of the building façade as independent masses separated by the niches. This castellation of the façade is very distinct from the 111 Building which has no relief in either the face or top of its parapet wall. It also is more uniform and complete in its appearance from that of the 222 Building which, while it has a dressed up front façade, reverts to exposed penthouse boxes on the backside of the building.
2. We have modified the glass parapet from the frosted glass in our original design, to nearly clear glass which is also different than any other building in the downtown, including 111, and 222.
3. We have suggested a subtle pattern be included on the face of the penthouse so that it becomes an integral part of the design, not just a mechanical box we are trying to hide. The actual pattern (shown as vertical grey shapes in the rendering) may take more time to fully develop, but we have shown a concept that integrates the vertical lines of the top of the building in an artistic vertical expression.

We think that these modifications combined with the unique sculptural shape of the building comply with the City's ordinance requiring that "the roofline contains architectural features that give it a distinctive form or skyline". No other building in the downtown has a clear glass parapet, no other building has the same deep modulations in the façade and top of parapet wall, and no other building has a penthouse that wants to be expressed rather than concealed. The owner has accepted these improvements with some hesitation and at the same time it may not go as far as Doug Dansie would have liked to see so I think it is a good compromise that improves the massing and appearance of the building.

I hope that you can offer a positive recommendation for this approach to the Planning Commission.  
Thanks,  
Dave Dixon

Phone: 801-595-6400



## **ATTACHMENT E: Existing Conditions**

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## **Existing Conditions:**

The site is currently vacant of buildings.

The adjacent uses include:

North: surface parking lot for the Federal Building (located at the corner of State and 100 South Streets.

East: multi-level parking structure that provides shared parking for some surrounding uses, and this proposed office tower.

South: Financial Bank

West: State Street

The Downtown Master Plan references the city's "Urban Design Element" standards for this core area, which encompasses the subject parcel. The design standards call for the downtown's "height" to be concentrated in the Central Business District, to continue to distinguish it as the core of the city. The proposed building complies with this standard, but as yet does not provide a distinct roofline to enhance and provide more interest to the skyline of downtown.

## **ATTACHMENT F: ANALYSIS OF STANDARDS**

**21a.59.060: Standards for Design Review:** In addition to standards provided in other sections of this title for specific types of approval, the following standards shall be applied to all applications for design review:

Standard	Finding	Rationale
<b>A. Development shall be primarily oriented to the street, not an interior courtyard or parking lot.</b>	<b>Complies</b>	The building design is primarily oriented to State Street.
<b>B. Primary access shall be oriented to the pedestrian and mass transit.</b>	<b>Complies</b>	The building has a number of entrances, the primary one facing State Street, which is a primary bus route.
<b>C. Building facades shall include detailing and glass in sufficient quantities to facilitate pedestrian interest and interaction.</b>	<b>Complies</b>	The building is predominantly glass and the ground level glass is clear, looking into a large lobby area, which facilitates pedestrian interest and interaction.
<b>D. Architectural detailing shall be included on the ground floor to emphasize the pedestrian level of the building.</b>	<b>Complies</b>	The ground level has design elements to emphasize the pedestrian. These elements differentiate the first three floors from the rest of the building for improved pedestrian interaction and access.
<b>E. Parking lots shall be appropriately screened and landscaped to minimize their impact on adjacent neighborhoods. Parking lot lighting shall be shielded to eliminate excessive glare or light into adjacent neighborhoods.</b>	<b>Complies</b>	No parking is proposed to occur on site, but rather will be provided in a new parking structure to the east, and a small number of stalls (11) to the north of federal property. The parking structure will be blocked from State Street by this new building, and the north parking area will be improved with additional landscaping and a pedestrian walk that is part of the mid-block walkway.
<b>F. Parking and on site circulation shall be provided with an emphasis on making safe pedestrian connections to the street or other pedestrian facilities.</b>	<b>Complies</b>	With this project, vehicle access will be revised in coordination with UDOT, to reduced pedestrian/vehicle conflict. The vehicle drive, location on the adjacent property to the north, will provide access to the drop-off area, the small parking lot, and the parking structure east of this building. Pedestrian walkways will be provided through the block and provide direct access to/from all parking areas.
<b>G. Dumpsters and loading docks shall be appropriately screened or located within the structure.</b>	<b>Complies</b>	Dumpsters and loading areas are located behind the building and appropriately screened.
<b>H. Signage shall emphasize the pedestrian/mass transit orientation.</b>	<b>Complies</b>	Application information indicates signage oriented toward the pedestrian and mass transit riders.
<b>I. Lighting shall meet the lighting levels and design requirements set forth in <a href="#">chapter 4</a> of the Salt Lake City lighting master plan dated May 2006.</b>	<b>Complies</b>	Application information indicates compliance with city's lighting standards, with cut-off lighting features.

<p><b>J. Streetscape improvements shall be provided as follows:</b></p> <ol style="list-style-type: none"> <li><b>1. One street tree chosen from the street tree list consistent with the city's urban forestry guidelines and with the approval of the city's urban forester shall be placed for each thirty feet (30') of property frontage on a street. Existing street trees removed as the result of a development project shall be replaced by the developer with trees approved by the city's urban forester.</b></li> <li><b>2. Landscaping material shall be selected that will assure eighty percent (80%) ground coverage occurs within three (3) years.</b></li> <li><b>3. Hardscape (paving material) shall be utilized to designate public spaces. Permitted materials include unit masonry, scored and colored concrete, grasscrete, or combinations of the above.</b></li> <li><b>4. Outdoor storage areas shall be screened from view from adjacent public rights of way. Loading facilities shall be screened and buffered when adjacent to residentially zoned land and any public street.</b></li> <li><b>5. Landscaping design shall include a variety of deciduous and/or evergreen trees, and shrubs and flowering plant species well adapted to the local climate.</b></li> </ol>	<p><b>Complies</b></p>	<p>Application indicates streetscape and landscape improvements will be installed to comply with these standards.</p>
<p><b>K. The following additional standards shall apply to any large scale developments with a gross floor area exceeding sixty thousand (60,000) square feet:</b></p>		
<ol style="list-style-type: none"> <li><b>1. The orientation and scale of the development shall conform to the following requirements:</b> <ol style="list-style-type: none"> <li><b>a. Large building masses shall be divided into heights and sizes that relate to human scale by incorporating changes in building mass or direction, sheltering roofs, a distinct pattern of divisions on surfaces, windows, trees, and small scale lighting.</b></li> <li><b>b. No new buildings or contiguous groups of buildings shall exceed a combined contiguous building length of three hundred feet (300').</b></li> </ol> </li> </ol>	<p><b>Complies</b></p>	<p>The building mass at ground level is divided into various angles, mass, and patterns and roof overhangs. Adequate landscaping is shown in front, and an accessible courtyard in back of the building provides some seating.</p> <p>The proposed building will not exceed 300 feet.</p>

<p><b>2. Public spaces shall be provided as follows:</b></p> <p><b>a. One square foot of plaza, park, or public space shall be required for every ten (10) square feet of gross building floor area.</b></p> <p><b>b. Plazas or public spaces shall incorporate at least three (3) of the five (5) following elements:</b></p> <p>(1) Sitting space of at least one sitting space for each two hundred fifty (250) square feet shall be included in the plaza. Seating shall be a minimum of sixteen inches (16") in height and thirty inches (30") in width. Ledge benches shall have a minimum depth of thirty inches (30");</p> <p>(2) A mixture of areas that provide shade;</p> <p>(3) Trees in proportion to the space at a minimum of one tree per eight hundred (800) square feet, at least two inch (2") caliper when planted;</p> <p>(4) Water features or public art; and/or</p> <p>(5) Outdoor eating areas.</p>	<p><b>Complies, Not Applicable</b></p>	<p>Design review criteria apply to multiple zoning districts city wide. This particular criterion is largely meant for big-box retail in a shopping center format. The D-1 zoning district allows for buildings to occupy the entire site, property line to property line. The D-1 zoning takes precedence. The building generally meets the intent of this standard by providing a large lobby; open to the public, and a courtyard between the office tower and the parking structure. Landscaping is consistent with downtown public way landscaping requirements.</p>
<p><b>L. Any new development shall comply with the intent of the purpose statement of the zoning district and specific design regulations found within the zoning district in which the project is located as well as adopted master plan policies, the city's adopted "urban design element" and design guidelines governing the specific area of the proposed development. Where there is a conflict between the standards found in this section and other adopted plans and regulations, the more restrictive regulations shall control.</b></p>	<p><b>Partially Complies (skyline issue)</b></p>	<p>The building responds to State Street and 100 South Streets with an urban format with no additional setbacks, as required by the D-1 zoning district. The urban design element encourages a sculpting of building on the skyline to encourage the creation of interesting skyline features. Height, scale, and character are significant building features of the downtown district, and should be treated as such. This particular building has some minor architectural detailing at the roofline, such as a slightly modulated parapet, clear glass, and a visual design on the penthouse that is visible through the glass, but is not sufficiently distinguished visually from other recent glass buildings with glass parapets at 222 S Main and 111 S Main. The basic design of this building is a glass tower with angular features and cut-outs as part of the glass façade. It may be argued that overly complicated architectural elements at the roofline would not be consistent with the general architecture of the building. The architects prefer continuing the basic architecture of the building to the skyline, but using cutouts and the mechanical penthouse as a method of creating interest at the skyline level, although staff does not find this effective in meeting the height standards as discussed in the Key Issues.</p>

**21a.59.065: Standards for Design Review for Height:** In addition to standards provided in 21A.59.060 (above), the following standards shall be applied to all applications to all applications for conditional building and site design review regarding height:

Standard	Finding	Rationale
<p><b>A. The roofline contains architectural features that give it a distinctive form or skyline, or the rooftop is</b></p>	<p><b>Does Not Comply</b></p>	<p>This rooftop is not designed for use by occupants or the public, and therefore is primarily subject to the first part of</p>

<p>designed for purposes such as rooftop gardens, common space for building occupants or the public, viewing platforms, shading or daylighting structures, renewable energy systems, heliports, and other similar uses, and provided that such uses are not otherwise prohibited.</p>		<p>the standard regarding “distinctive form or skyline”. As discussed on <b>Standard L</b> of the previous table, the proposed roofline consists of minor features such as a slightly modulated parapet and a visual design on the penthouse that is visible through the glass, but is not visually distinctive from other recent glass buildings with glass parapets at 222 S Main and 111 S Main. It may be argued that overly complicated architectural elements at the roofline would not be consistent with the general architecture of the building. The architects prefer continuing the basic architecture of the building to the skyline, but using cutouts and the mechanical penthouse as a method of creating interest at the skyline level. Staff does not find this effective in meeting this “distinctive” standard as discussed in the Key Issues.</p>
<p><b>B. There is architectural detailing at the cornice level, when appropriate to the architectural style of the building.</b></p>	<p><b>Complies, Not Applicable</b></p>	<p>The proposed building design does not incorporate a cornice but instead has a glass parapet at the top.</p>
<p><b>C. Lighting highlights the architectural detailing of the entire building but shall not exceed the maximum lighting standards as further described elsewhere in this title.</b></p>	<p><b>Partially Complies</b></p>	<p>Lighting is focused on the pedestrian level, as part of the open grand lobby and entrances. Staff is unaware of any other proposed lighting on the remaining exterior of the building, particularly at the higher levels. With the façade being glass, visible lighting will come primarily from the interior lights.</p>



## **ATTACHMENT G: Public Process and Comments**

## **Public Notice, Meetings, Comments**

The following is a list of public meetings that have been held, and other public input opportunities, related to the proposed project:

### **Notice of Application:**

A notice of application was mailed to all abutting property owners. The notice sought a reply from anyone who wanted a public hearing with the planning commission regarding this petition. Staff initially received three (3) separate requests for a public hearing; however one was retracted after meeting with the architect to discuss the project in more detail. The other two respondents continue with their request for a public hearing, citing concerns with the height and its potential affect on the properties directly across State street (on the west side).

### **Notice of the public hearing for the proposal included:**

Public hearing notice mailed on April 10, 2014

Public hearing notice posted on April 11, 2014

Public notice posted on City and State websites and Planning Division list serve: April 10, 2014

### **Public Comments**

The applicant presented the proposal two times to the Capitol Hill Community Council, in April and May, 2013. A copy of the community council's written comments are included with this report as "Attachment E". Some members of the community are concerned about the number of units and visitor parking for this development. They are opposed to utilizing street parking for the visitors. No other public comments were received prior to the completion of this report. Comments received after will be provided to the planning commission members at the meeting.

## **ATTACHMENT H: Department Comments**



## CITY DEPARTMENT COMMENTS

**Public Utilities** (Justin Stoker):

Applicant should refer to DRT notes for guidance in the development process. Project details will be reviewed during the permitting process when design has been completed.

**Engineering** (Scott Weiler):

Certified address is required prior to obtaining a Building Permit. See Alice Montoya at 801-535-7248. UDOT approval and permit is required for proposed drive approach location on State Street. Approved site plan is required from SLC Engineering. The new sidewalk cross slope must be designed at 2% max. cross slope to comply with ADA. Decorative sidewalk meeting the CBD Sidewalk Design Standard is required. The tree species must be approved by the Urban Forester, with tree grates that comply with ADA. Submit site plan to SLC Engineering Permits Office @ 349 South 200 East for review. Contact George Ott @ 801-535-6396 for Permit information. Contact Michael Clara at UTA for UTA Bus Stop approval (#801-262-5626 or 801-287-2325). A Public Way Permit is required from SLC Engineering prior to commencing the work in the public way behind curb on State Street. Licensed, bonded and insured contractor to obtain Public Way Permit from SLC Engineering.

**Transportation** (Barry Walsh): The Project description document notes the existing parking lot 139 South revision and agreements for cross access and parking with the Federal owner, along with the coordination with UDOT for the State Street drive way revisions. The notation of 2.5 stall per 1,000 sf is a general statement and per the DRT comments a full parking calculation and offsite parking lease agreement will be required, to comply with City standards.

(Prior comments from Transportation, still applicable): Preliminary discussion to outline new 18 story building development on one lot with off site parking lease agreements. Provide parking calculations per section 21A.44.060 etc. Address ADA, carpool, electric vehicle stalls, bicycle stalls, loading dock, etc. Also discussed revision of existing Federal parking lot at 139 S State (separate permit) and coordination with UDOT for revision to drive approach. Coordinate with Planning for public sidewalk paving pattern. Coordinate with UTA for existing bus stop. Note the second phase of the Parking structure for completion prior too, or in conjunction with, in order to provide the parking for the off site lease agreements.

**Zoning:** (Ken Brown):

This proposal is to be consistent with the Standards For Design Review and Standards For Design Review For Height as noted in 21A.59. This proposal has not been reviewed for compliance to percentage or type of first floor glass, compliance to minimum and maximum parking requirements, screening of service areas, midblock walk ways or landscaping requirements; except to note that the Boyer 151 Tower Project Description indicates that 6 street trees will be provided and only 5 are shown.

## **ATTACHMENT I: Motions**

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## Potential Motions

Based on the findings listed in the staff report, it is the Planning Staff's opinion that the project adequately meets the applicable standards for a planned development and preliminary subdivision plat and therefore recommends the Planning Commission approve the application as proposed and subject to the following:

### **Staff Recommendation:**

Based on the findings listed in the staff report and the testimony and plans presented, I move that the Planning Commission deny the requested Tower 151 Conditional Building and Site Design Review PLNPCM2014-00045 to allow extra building height as proposed.

### **Not Consistent with Staff Recommendation:**

Based on the testimony, plans presented and the following findings, I move that the Planning Commission approve the requested Tower 151 Conditional Building and Site Design Review PLNPCM2014-00045 to allow extra building height subject to the following conditions:

1. Prior to issuance of any permit to begin construction of the building, the applicant/owner shall verify to the satisfaction of the Planning Director, that the agreements and cross access easements are in place with the federal government (property to the north) claimed by the applicant regarding the "no build" easement, and parking lot, landscaping, and walkway as shown on the most recent application site plan.
2. Prior to final occupancy approval, the applicant/owner shall verify that the parking garage on the property to the west, upon which parking for this project was approved, is completed and ready for use.
3. The applicant shall comply with all other zoning and building requirements applicable to the project.
4. The applicant/owner shall install all required public way improvements.

The Planning Commission shall make findings on the conditional building and site design review standards and specifically state which standard or standards are being complied with.